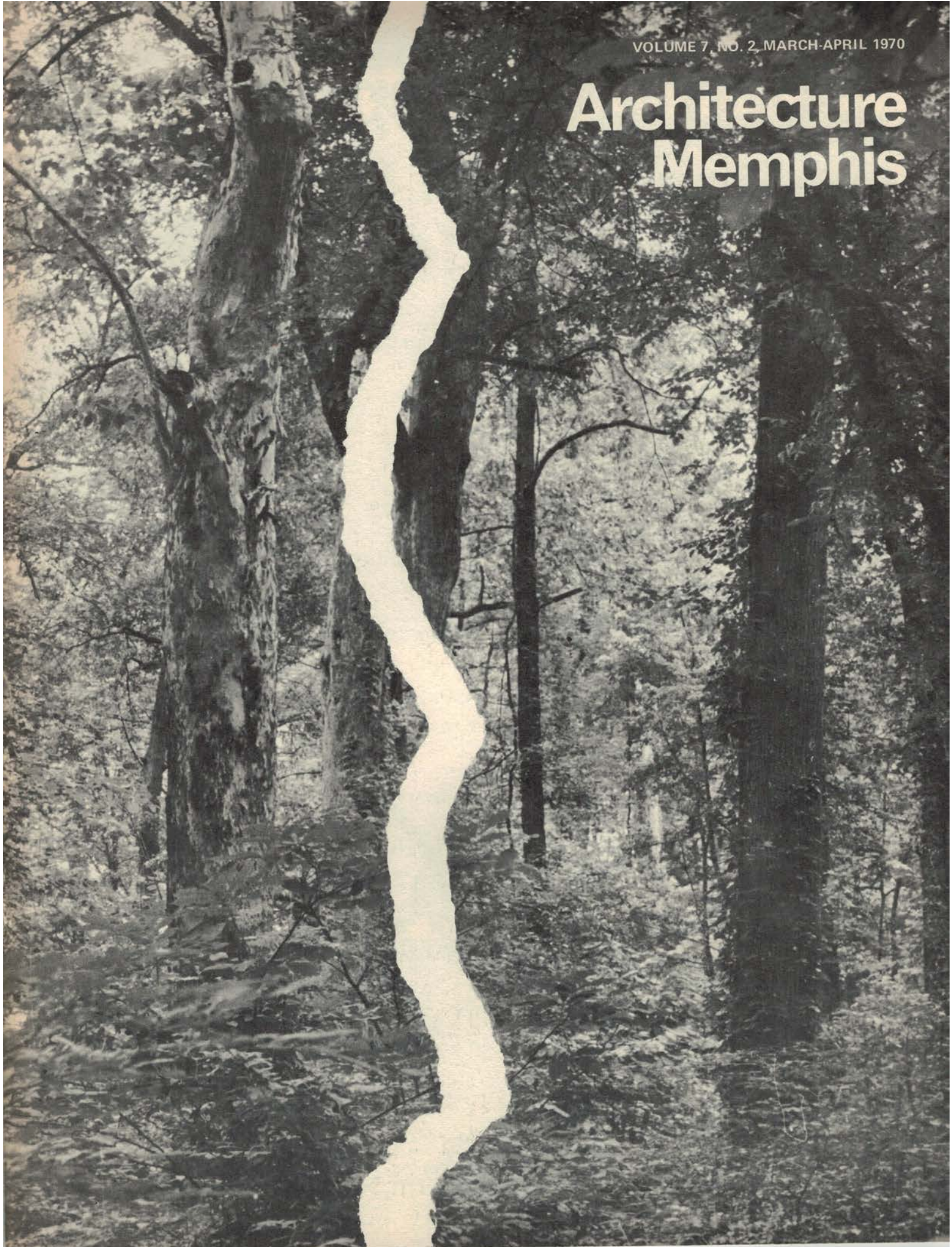


VOLUME 7, NO. 2, MARCH-APRIL 1970

Architecture Memphis



Span-Deck® speeds construction of new U-T Martin dormitory

More than 30,000 sq. ft. of Span-Deck pre-cast concrete planking were manufactured by Shelby Precasting Corp. for the University of Tennessee's \$1,093,000, 92-suite married students dormitory at Martin. Span-Deck was selected for the project because it is easy to handle, quick to erect and aesthetically pleasing. Span-Deck also helped maintain construction schedules since it is produced in the Shelby Precasting plant and unaffected by job-site weather conditions.

Another recent application of this modern structural product is the new dormitory complex at

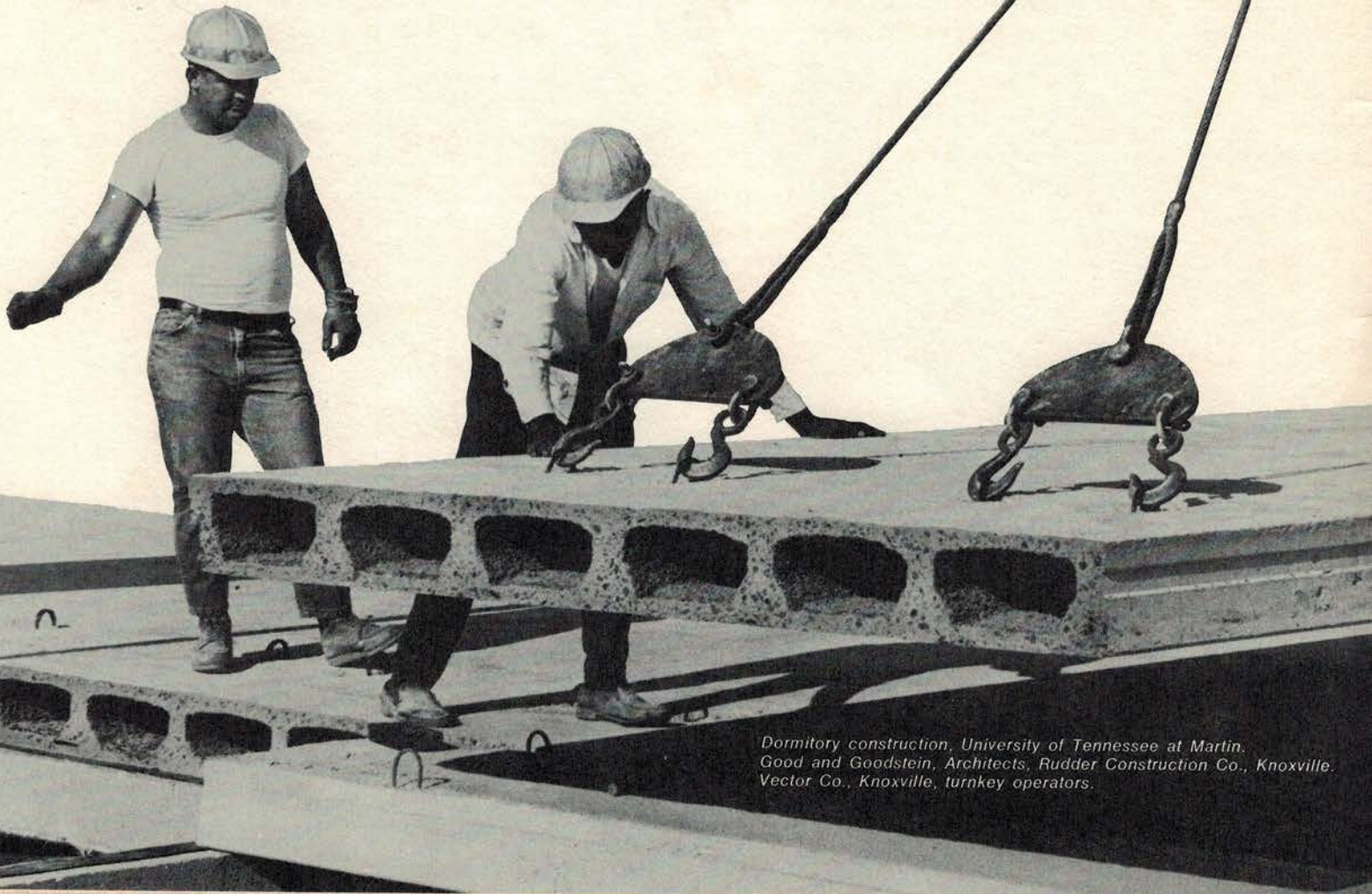
Mary Holmes Junior College, West Point, Miss., utilizing 103,000 sq. ft. of Span-Deck from Shelby Precasting.

Designers and builders throughout this area are becoming increasingly aware of the important benefits Span-Deck affords. It is ideal for floors, wall panels, ceilings and roofs. Span-Deck comes in 8' wide by 8" or 12" deep sections, cut to specified length and numbered for order of erection. In addition, Span-Deck has special fire-retarding qualities that can result in insurance savings. For more information, call or write



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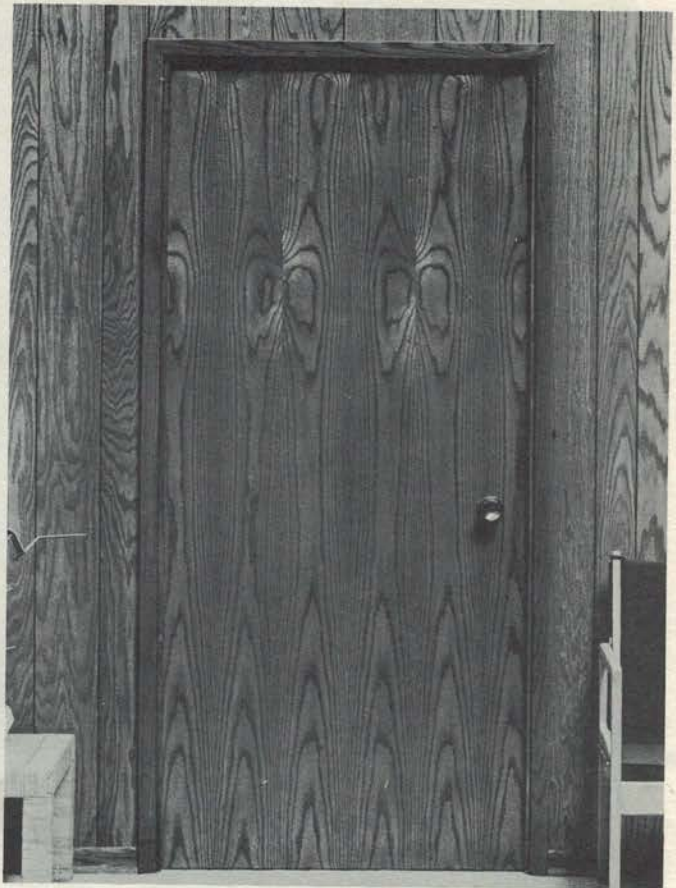


*Dormitory construction, University of Tennessee at Martin.
Good and Goodstein, Architects, Rudder Construction Co., Knoxville.
Vector Co., Knoxville, turnkey operators.*

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Solid Staved Core



Solid Wood Flake Core

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Yes	Guarantee Interior Lifetime	Yes
Yes	2-Year Exterior	Yes
	Special Resin-impregnated fiber sheet in place of normal crossbanding on walnut doors. (Eliminates light line. Gives door edge uniform appearance.)	
No		Yes
Yes	Telegraphing	No

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It does not telegraph. And it costs less than the solid stave core door.

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Vista del Rio, recipient of Kansas City Chapter's A.I.A. Medal of Award, is a 250-unit apartment structure built by the Kansas City Education Association's Housing Foundation, Inc., for occupancy by the retired teachers of Kansas City. The 20-story building includes complete dining, infirmary, library, arts-crafts, recreational and activity spaces as well as underground parking. Dover manufactured the complete vertical transportation system, from hoisting equipment through decorative cabs. Offering both electric traction elevators for high-rise buildings and Oilraulic® elevators for buildings to six stories, Dover has the capability to give you the best combination of equipment for any building. And Dover delivers the engineering assistance you require for plans and specifications. Write for catalogs, or see Sweet's Files. Dover Corporation, Elevator Division, Dept. U-1, P. O. Box 2177, Memphis, Tenn. 38102. In Canada: Dover/Turnbull.



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Architecture Memphis

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Congress, when it passed Public Law 89-670 establishing the Department of Transportation, dictated that "special effort should be made to preserve the natural beauty of the countryside and public parks and recreation lands, wildlife and waterfowl refuges and historic sites", and in the Federal Aid Highway Act of 1966, Congress declared that "a maximum effort be made to preserve Federal, State and local parklands and historic sites, and the beauty and historic value of such lands and sites."

If this is the expressed will of Congress, the question arises: Why is it that Overton Park, an acknowledged civic treasure, is about to be despoiled by the legalized vandalism of the highwayman? The answer is complex, but in part it is the hope of pumping dollars into a moribund downtown that has created an establishment hysteria eager to damn the park and build the expressway.

At the forefront of this drive for self-destruction, anxious to protect the advertising revenues derived from downtown mercantile sources, is the Scripps Howard controlled news media. Scripps Howard in Memphis, with its two newspapers, and a radio and television station, has effectively censored and blacked out the pro-conservation sentiment that exists in the community. It has aligned itself with regressive political leadership to stifle and ridicule those who would protect our natural heritage. It has painted the cause of preservation as a hopeless waste of time, and the leaders of Citizens to Preserve Overton Park as anti-progress, bleeding hearted dogooders.

The result is that the bulldozers are at the gates of the park and the earth moving machines are waiting to cut a swath of destruction through the virgin forest. Memphis has been told by the *Press Scimitar* that—"the loss is nothing as bad as it's been painted. Some large trees will be felled, but mostly the areas to be used for the expressway bear scrubby trees and bushes." To this we can only ask, what happened to the ideals of the late Edward J. Meeman?

The Memphis Chapter AIA can take little credit for helping to preserve the park. It too has been told that the die is cast and that the park's demise is inevitable, and for this we are sorry. But even now, more expressways are being planned and doubtless other civic treasures will be affected, such as our historic relation to the Mississippi River. Other communities in our nation have begun to realize that through imaginative architectural design, adequate funding and proper governmental coordination, an expressway can be a positive addition to the city. We urge that Memphis adopt this enlightened policy, and we implore our civic leaders and the administrators of the Highway Program to resolve that we shall never again face such a dismal situation.

Overton Park was purchased in November 1901 from Overton Lea and his wife by the City of Memphis. A right-of-way had existed from 1892 when the Leas sold a 50 foot strip thru the woodland to the Raleigh Springs Railroad to be used for transportation to the springs. This right-of-way still exists and is used by the Memphis Transit Authority. It is well planted and wooded on both sides, and therefore not too much of an intrusion. In 1902, the people, by popular vote, named the park in honor of a city founder and benefactor, Judge John Overton. The park has always been very much in the center of recreational and cultural life for Memphians, and a source of pride for almost 70 years.





The park comprises 342 acres of land of which approximately 170 acres are Oak-Hickory climax deciduous forest, in which typical flora and fauna abounds. Its relatively undisturbed forest floor is covered with rotting logs, understory trees, shrubs and herbs. The size of the forest is large enough to encourage stop over of many species of migratory land birds in both spring and fall, and, there is no other location within less than an hour's drive where one might observe these transient visitors from northern habitats.

For many years naturalists have kept daily records of arrivals and departures of migratory and residential birds, and as a science project, two high school students reported a two year study of bird species found daily in the park, winning a gold ribbon. The nearness of the native woodland to the Memphis Zoo makes coordinated teaching of plant and animal life a unique experience not usually accessible to city dwellers.

An expressway will reduce the wooded area sufficiently to ruin it as a natural area suitable for sheltering wildlife, and for the promotion of ecological and conservational education.





"Overton Park with its natural beauty is a priceless possession of the people of this city. Its value cannot be measured in dollars and cents. It is hallowed ground—our heritage to our children."

Former Mayor Watkins Overton

"Beauty is not an easy thing to measure—it is one of the most important components of our true national income, not to be left out simply because statisticians cannot calculate its worth."

President Lyndon B. Johnson

"I am opposed to highways going through any park—city, state or national."

Former Secretary of Interior Stuart Udall

"There is too much evidence in cities across America that in selecting the corridor and constructing the highway we have overlooked what the highway is supposed to serve. We have destroyed whole neighborhoods, ruined or buried parks and waterfronts, and displaced entire communities without exploring all possibilities."

Morris Ketchum, FAIA
Past President, AIA

"The issue at places like Overton Park affords an opportunity to demonstrate that democracy is not a government in behalf of special interests, but a government in behalf of the peoples' interest."

Mike Frome
Conservationist and Author



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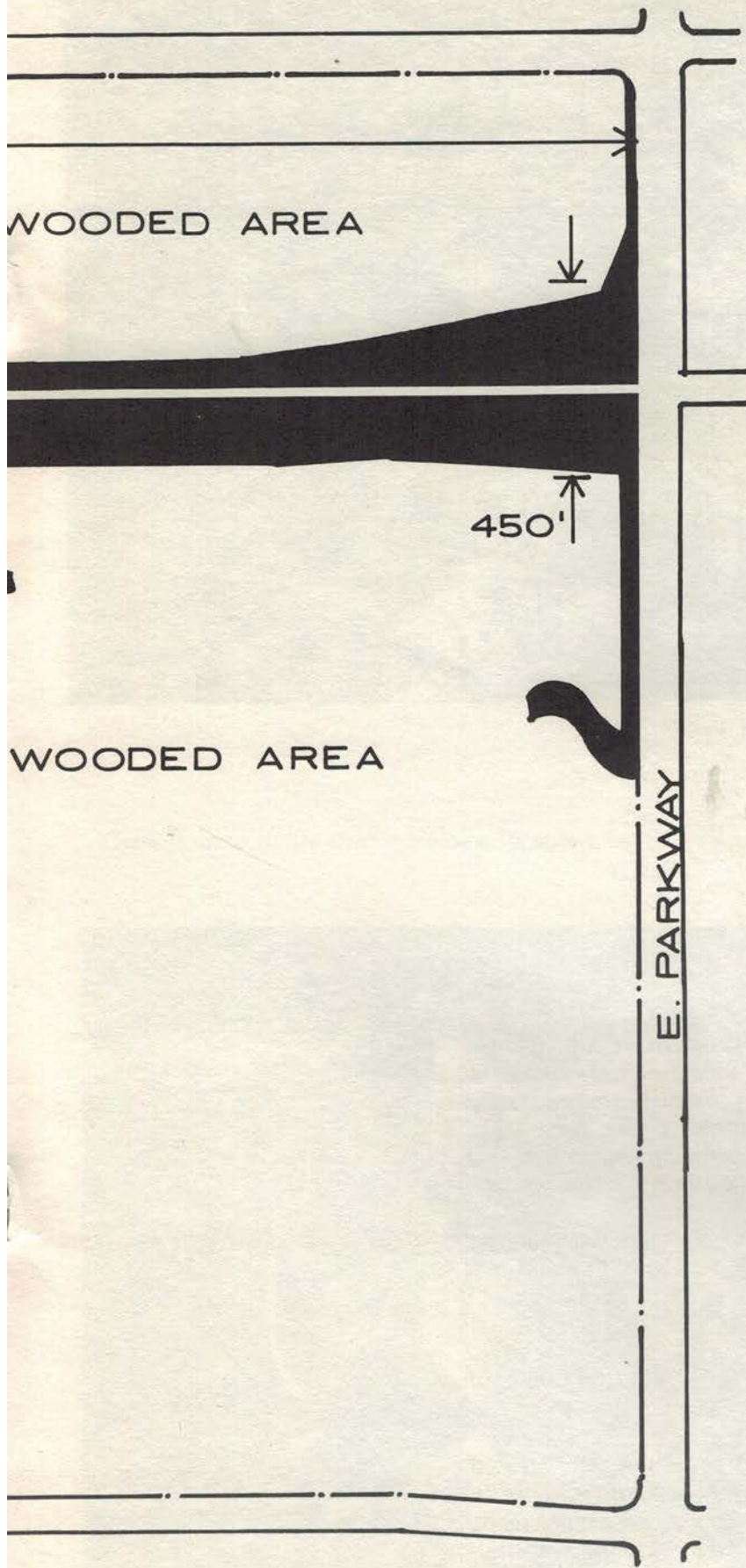
Overton Park

GOLF COURSE

NORTH



POPLAR AVENUE



The area printed in black is the right-of-way that the expressway will require. It will use approximately 22 acres of ground or roughly 7 per cent of the total park area. The white line in the middle of the black area is the present 30 foot wide paved area used exclusively by the Memphis Transit Authority.



"There is a new philosophy underlying the highway program. It is predicated on the fact that social responsibility is an important aspect of highway building. We must constantly ask ourselves if we are seeking to satisfy transportation needs at the expense of other vital needs of the American people. We must be concerned with the effect highways may have on the people living on the areas traversed, on their neighborhoods, their environment, their institutions and their resources."

Ralph Bartelsmeyer
Director of the U. S. Bureau of Public Roads

"We shape our buildings, and then our buildings shape us."

Winston Churchill

"Many of the problems that today plague our suburbs and cities were created because the decisions that shaped them were made singly, without reference to other decisions. Our concern is to make sure that all the many decisions made on various governmental levels be coordinated so that a unified, healthy, efficient environment will result."

William L. Slayton
Executive Vice President, AIA

"We need a plan to bring joy, life and happiness to our cities."

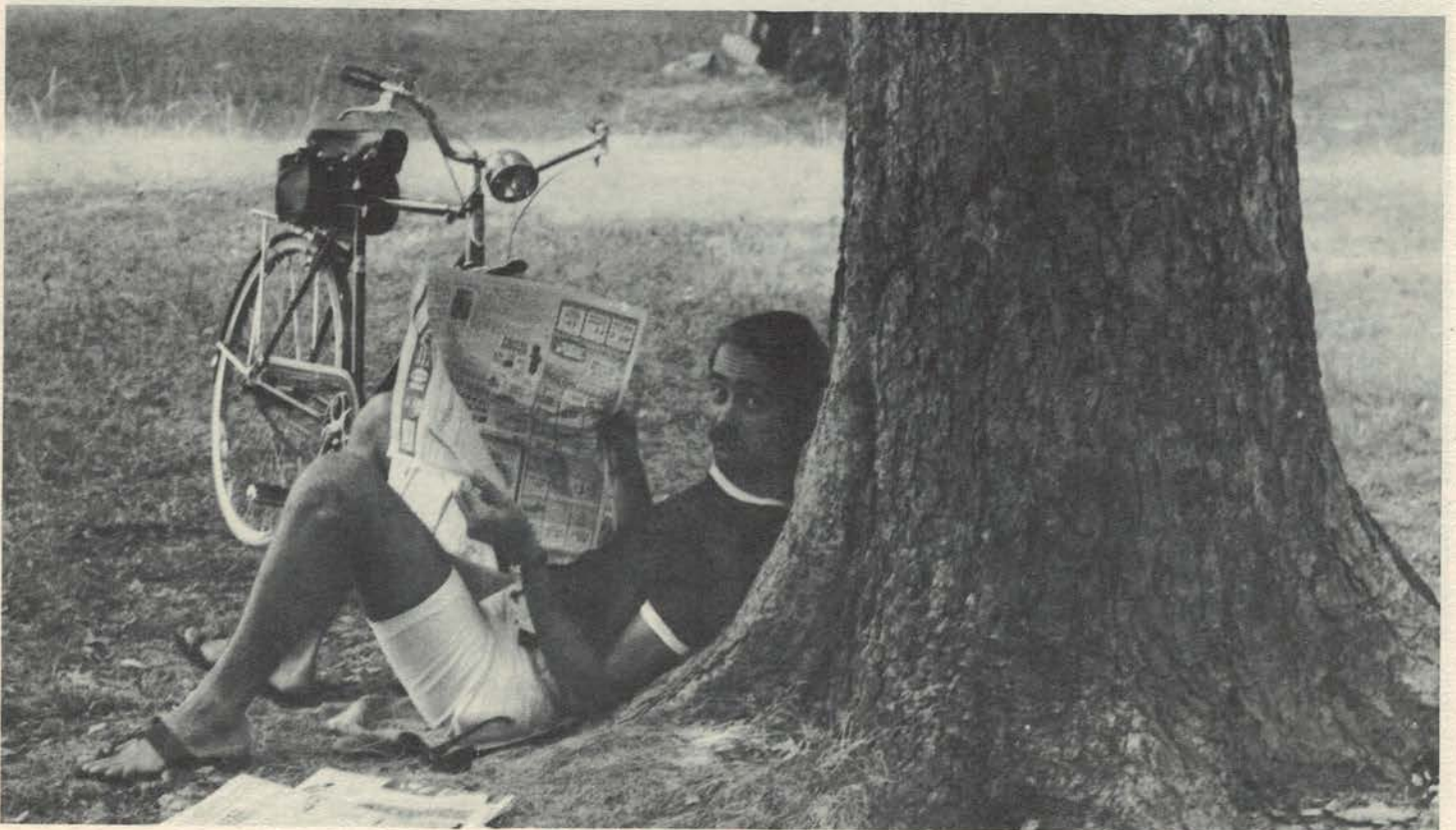
M. Paul Friedburg
Landscape Architect

"The civilized man has built a coach, but he has lost the use of his feet."

Ralph Waldo Emerson

"There is more to life than increasing its speed."

Gandhi





A·M REPORTS

ARCHITECTS ASK CHANGE IN NEXT FEDERAL HIGHWAY PROGRAM

William L. Slayton, Executive Vice President of AIA, testifying before the House Committee on Public Works, told Congress that America's next generation of Federal highways should concentrate on metropolitan areas and provide a unified trust fund for roads and mass transit. He recommended these changes in the planning, financing, and construction of the Post-Interstate Highway system:

*One transportation trust fund should be generated from user taxes and other sources to support construction of highways, subways, bus and rail rapid transit.

*"Substantially" greater percentages of Federal highway funds should go to meet needs of urban areas.

*Where appropriate, "future urban expressways should include space within the right-of-way for a mass transit system" which could be either rail or bus.

*New park-and-ride strategies, new transit technologies, and new ways to move pedestrians are essential in crowded urban cores and should be encouraged by funds from the next major national transportation act.

*Local political jurisdictions should determine location of highway and transit links and should have a major role in their design.

*Transportation Corridor Districts should be created so metropolitan areas could plan and control land near and affected by the new highways and transit links. As land develops around the transportation corridor, part of the profit from increased land value would be returned to the public treasury. In effect, such land development would help finance the transportation system.

*Interdisciplinary teams of architects, engineers, landscape architects, and other design professionals should work on new major urban transportation systems, including highways, to minimize disruption of the fabric of city life and to build joint uses which promote a prosperous and livable city.

*Highway funds should include money to construct housing, so that citizens in the path of the new roads can be relocated and a city's housing supply will not be cut. Before highway construction starts, states and cities should determine how much replacement housing is needed and whether it can be supplied by the local private and public market without special Federal help.

*At least two local public hearings should be required by Congress for new highways to give citizens an opportunity to discuss the basic route decision, then review plans later.

*Federal law requiring "just compensation" for billboards near Interstate links should be repealed to allow local jurisdictions to use their police and other powers to control signs. Highway beautification has been brought "to a standstill" because of lack of funds and the "just compensation" provision, said AIA.

*Three percent of appropriated highway funds should be used for landscaping and scenic enhancement to help make the routes "an object of civic pride."

*States need also to give continuing stress to quality and variety in materials which can improve the appearance of new highways. Such details as road textures, bridge forms, lighting "must be given special attention."

*All Federally-aided transportation projects should be reviewed by the newly created Office of Urban Systems and Environment to minimize damage to the environment.

Office Profile

MEL O'BRIEN/ASSOCIATES/ARCHITECTS



PICKWICK STATE PARK INN/BEALE STREET BLUE LIGHT DISTRICT/RIDGEWAY COUNTRY

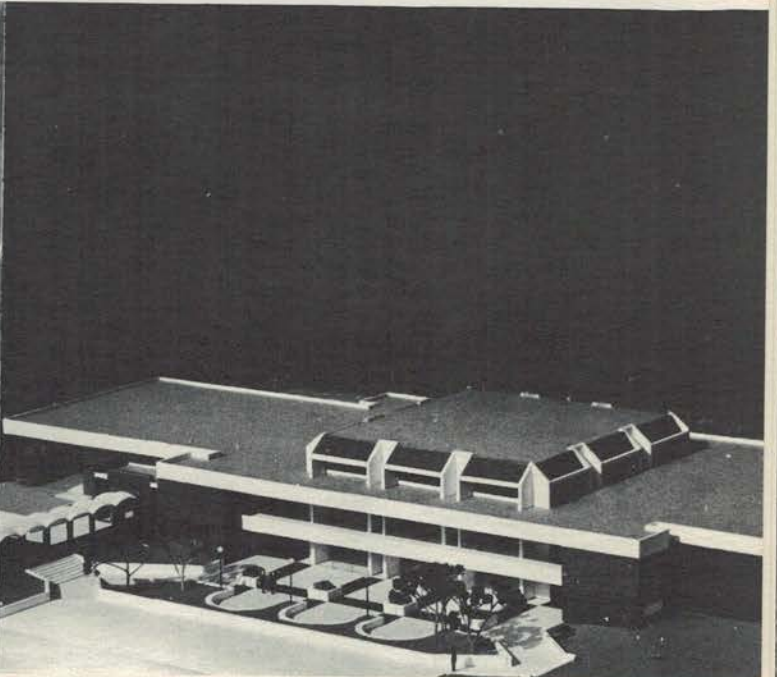
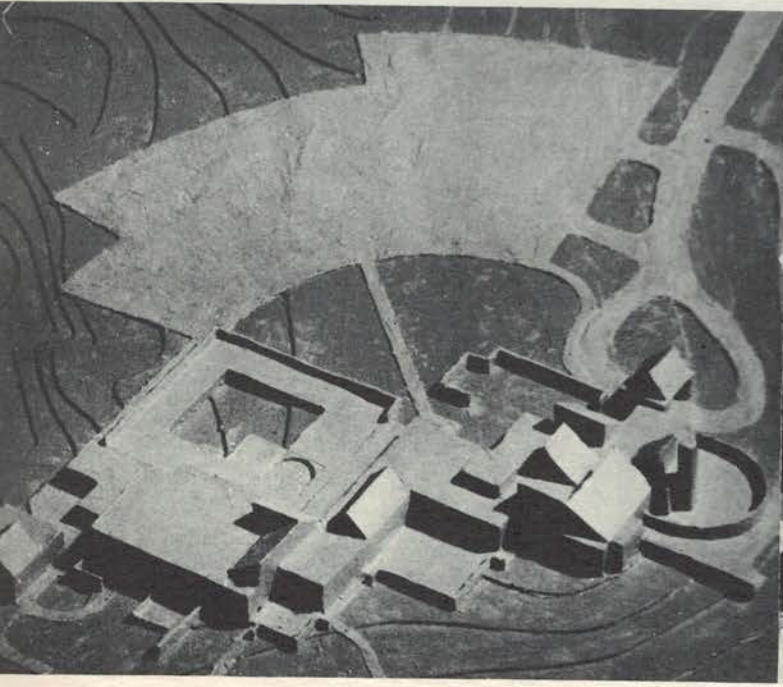




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Students Page

STUDENTS

The newly accredited School of Architecture at the University of Tennessee is for the first time eligible to compete for the Reynolds Aluminum prize. The local winning entry from the School was submitted by Charles D. Smith, a third year design student from Chattanooga, Tennessee. His submission was a system of pedestrian circulation components. The flexibility of this component system was intended to solve a number of pedestrian auto conflicts evident in most urban areas.

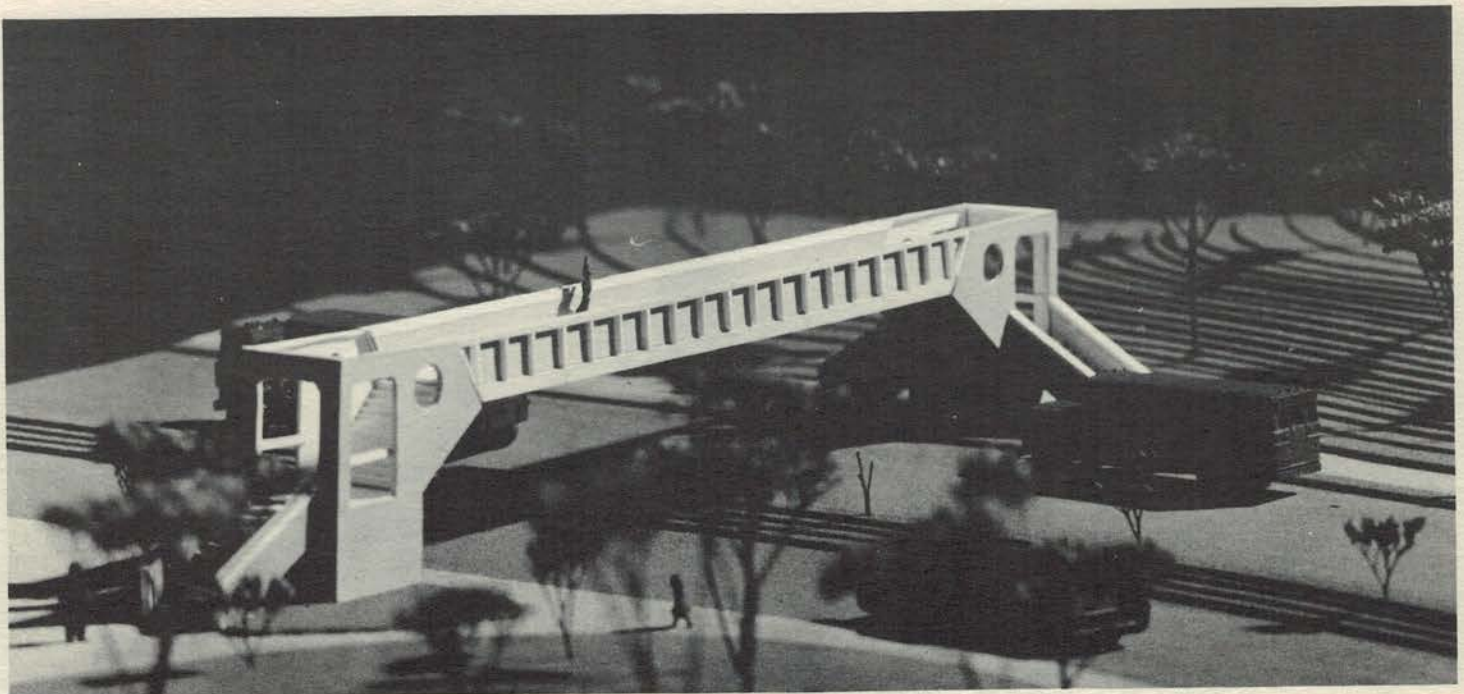
The basic components include: (1) A twelve foot horizontal module with a spanning capacity up to 120 feet. (2) A column which receives the horizontal members and provides a transition from horizontal to vertical movement. Openings on 3 sides of the column receive the stair modules. (3) The stair component is designed to plug into the column openings. Where stairs are not located in the column opening, handrails are inserted.

The advantages of the system over conventional pedestrian bridges are:

- A. All Components are pre-fabricated.
- B. Components can easily be shipped to the site.
- C. Being Aluminum, the components are lightweight and weather resistant.
- D. The components are easily and quickly bolted together requiring a minimum of traffic stoppage.
- E. The components can be inventoried by a community or local government.
- F. The system can be disassembled and relocated.



Charles D. Smith



TSA

Convention News

The second annual Tennessee Society of Architects convention is scheduled for September 3-5 in Nashville. Convention headquarters is the Sheraton Motor Inn.

Some of the speakers:

Louis De Moll, AIA, will discuss the Institute's "Future of the Profession Report." He is a member of the AIA committee which conducted an in-depth survey of the profession. The full report will be released shortly. De Moll's appearance will be the best chance the majority of Tennessee's architects have to hear this report discussed by an authority.

John Wright, FAIA, will discuss Institute governmental affairs programs. An articulate speaker, Wright is thoroughly familiar with this subject. He will show those in attendance why it is in their best professional and financial interests to get involved in local government.

Paul B. Farrell, Jr., is an architect, attorney, planner, and mortgage banker. He has authored articles in the May 1969 and May 1970 editions of *Progressive Architecture*. These articles—and his presentation—do not share the same views of the architect's future as does the Octagon.

The convention will offer varied social functions. One is a "Casey Jones" party at Union Station.

Building products displays will again be the financial backbone of the convention. Over 40 exhibits will be on display.



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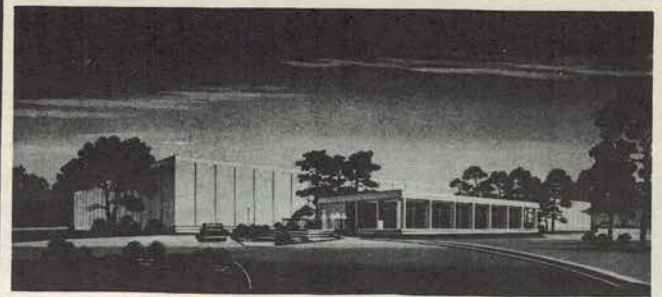
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Artists and Memphis

Ron Jerit has been associated with theatre art for the past twelve years both as a teacher and designer at the Universities of Illinois, New York, Florida and Memphis State and more recently as design consultant on numerous university and community theatres through his firm, General Theatre Consulting. Among the theatres he has consulted on in this area are: Memphis Little Theatre; Memphis State Fine Arts Complex; Southwestern Fine Art Building; Jackson, Tenn., Civic Auditorium; Fine Arts Building of the University of Tennessee at Martin; and the Fine Arts Complex of Louisiana State Univ. at New Orleans.

The design challenge of a new theatre lies in being given unlimited raw material—space—and in attempting to convert that raw material into a theatre and related spaces which will bring out the best in an audience and theatre artisans. Contemporary theatre has heightened this challenge by breaking with conventional production and architectural forms and traditional aesthetics. The visionaries in the theatre have made demands which architects, designers, and manufacturers must solve in new materials and spatial forms. Essential to the search for such solutions is a thoroughgoing knowledge and a working proficiency in the many crafts of the theatre, from design to production. A solution is found when the symbiotic relationship between actor and theatre space invites the audience to “play house” in the illusory world of the theatre and the audience willing gives itself to an evolving dramatic action. Joined in this way, architectural spaces, the moving actor and the willing audience become inseparable.



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